

The Commercial Driveway Application Checklist included hereafter (a clear photocopy is permissible) shall accompany the application to facilitate review by the Public Works Department.

### DRIVEWAY APPLICATION CHECKLIST

Residential  Commercial

1. This property is legally described as Lot \_\_\_\_\_, Block \_\_\_\_\_,  
\_\_\_\_\_  
\_\_\_\_\_ Subdivision,  
Section \_\_\_\_\_, Township \_\_\_\_\_ North, Range \_\_\_\_\_ East, City of Hastings, Nebraska.
2. The commonly known street address: \_\_\_\_\_
3. Name and/or Use of property: \_\_\_\_\_
4. Driveway dimensions: (reference: *Figure D-2 Driveway Design and Location Standards*)
  - a. Distance from property line to centerline of driveway \_\_\_\_\_
  - b. Distance from closest side street curb to near edge of driveway \_\_\_\_\_  
(minimum 55')
  - c. Throat width of driveway approach (W) \_\_\_\_\_
  - d. Radii dimensions (R) \_\_\_\_\_
  - e. Total curb cut dimension (W + R + R) \_\_\_\_\_
5. Parking lot dimensions: (reference: *Figure PL-1 Parking Lot Design Standards*)
  - a. Angle of Parking \_\_\_\_\_
  - b. Stall Width \_\_\_\_\_
  - c. Stall Depth \_\_\_\_\_
  - d. Aisle Widths \_\_\_\_\_
6. Special Considerations:
  - a. Vehicle waiting storage requirements are: \_\_\_\_\_ (reference: *Table VS-1 Driveway Design and Location Standards*).
  - b. Sight distances: Speed \_\_\_\_\_ mph Required sight distance \_\_\_\_\_ feet  
(reference: *Figure SD-4 Appendix A*).

## Hastings City Code

### (6) General instructions for driveway permit application.

#### (a) General.

Application for driveway permits are made to the City of Hastings Development Service Department.

The application shall include the address and legal description of the property served by the proposed driveway approach, the property owner's name, length of proposed curb cuts, the name, signature and telephone number the representative of the property owner authorized to make decisions concerning the driveway approach binding on the property owner.

#### (b) Application for driveways serving single family and two-family dwellings.

Application for driveway permits for single and two-family dwelling units may be approved by the Development Services Department when it is determined that the site plan drawn on the application is in conformance with these standards and the regulations of the City. Only one such driveway may be approved for each dwelling unit unless the application meets the requirements for circular drives as illustrated in Figure D-1. One additional driveway for a single buildable lot may be approved on non-major streets when a written request and drawing as set forth herein is submitted indicating the justification for such additional drive and when the same is approved by the Director of Public Works.

#### (c) Application for driveways in conjunction with building activity.

Application for driveways in conjunction with building activities shall be filed at the time of application for the associated building permit. The issuance of the driveway permit and payment of fees however, need not coincide with the issuance of the building permit.

#### (d) Multi-family, commercial and industrial uses.

Applications for driveway permits for all driveways, other than single or two-family dwellings, shall include three (3) copies of a site plan which shows the following items:

1. Scale drawing (1" = 20') with north arrow indicated.
2. Property lines and setback lines of property served, with dimensions.
3. Building and structure lines (note overhead or drive-in door locations).
4. Parking lot layout, with dimensions of aisles and stalls.
5. Parking lot barriers.
6. Adjacent roadways and sidewalks.
7. The proposed driveways, with dimensions.
8. Location of physical features of the property (i.e. trees, poles, inlets, manholes, valves, utilities, existing drives or curb cut locations, and service connections, within the public right-of-way).
9. Proposed traffic volume and vehicle type using the driveways.
10. Curb shall be ground or total removal and replacement shall be determined by Street Superintendent or authorized representative.

(Ord. No. 4233-11/2009)

## Hastings City Code

This standard shall apply to all private roadways located within the City of within the zoning jurisdiction of the City of Hastings.

(Ord. No. 4233-11/2009)

### 38-405. Driveway design standards.

#### (1) General.

This standard shall apply to all residential and commercial driveways located within the City and commercial driveways located outside the City Limits but within the zoning jurisdiction of the City.

Construction of any driveway shall require and be subject to the conditions of a driveway permit issued through the Department of Development Services. Requirements for permit applications are covered herein.

#### (2) Policies.

##### (a) General.

It is the policy of the City of Hastings to promote the maximum safe and efficient travel of persons on the public right-of-way and to preserve the maximum capacity of the roadway to accommodate such travel.

The issuance, denial, modification and revocation of driveway approach permits and the ordering of the removal, reconstruction, relocation, or alteration of any driveway approach may be used to implement this policy.

Driveway approaches shall be designed so that under the specific conditions for the property:

(i) Reasonable access from the roadway is afforded.

(ii) Horizontal separation from other approaches and roadway intersections is the maximum attainable.

(iii) The area and number of points where conflicts between vehicles using the approach, through vehicles using the roadway and pedestrians using the sidewalks is kept to a minimum.

(iv) Speed differential between vehicles using the approach and vehicles on the roadway is kept as low as possible.

(v) The driver entering or leaving the approach has the maximum unobstructed view of other vehicles using the roadway.

(vi) The maximum safety and efficiency of the right and left turning vehicles using the approach is afforded.

(vii) The frequency at which vehicles must stop or substantially reduce speed on the roadway because of actions of vehicles entering or leaving the driveway approach is kept to a minimum.

(viii) The maximum safety, efficiency and capacity of the roadway is promoted.

##### (b) Costs of construction.

The Owner of the property served by the driveway approach shall pay all costs for constructing the driveway approach; required additional turn lanes; pavement widening; median construction or reconstruction; alteration of manholes, storm sewer inlets, water valves or fire hydrants; relocation of power poles or light poles; and alteration of any other public utilities affected by the construction of the driveway approach. Street Department will address asphalt replacement adjacent to driveway approach at no cost to applicant.

##### (c) Number of driveway approaches.

Guidelines for the number of driveway approaches to be permitted are as follows:

## Subdivisions

(i) For single and two-family dwelling units, only one driveway approach per dwelling will be permitted unless the application meets the requirements for a circular driveway as set forth in Figure D-1. One additional driveway approach for a property may be granted on a non-major street when a site drawing and written request indicating the justification for such additional drive is submitted and such request is approved by the Director of Public Works or designated representative.

(ii) Only one two-way commercial driveway approach or one pair of one-way commercial driveway approaches should be permitted to a property having less than 400 feet of frontage and taking access from a major street.

(iii) Commercial driveway access to a major street from adjoining properties having a total frontage of less than 200 feet should be consolidated where possible. Jointly used driveways are encouraged along major streets to obtain maximum spacing of driveway approaches. These joint use driveways should provide at least a 50 feet deep access easement between the property owners.

(iv) For property located on a corner, commercial driveway access should be limited to one driveway approach. If the property is located at the intersection of a major street and a local or collector street, the driveway approach should be located on the local or collector street to reduce potential conflicts. If desirable horizontal separation of the driveway approach from the intersection and other approaches is attainable, and the land use warrants additional access, an access to both streets may be permitted.

### (3) Design.

#### (a) General.

Driveways used for commercial or industrial purposes, except those leading to loading docks or vehicle access doors, shall be designed such that vehicles can leave and enter the roadway in a forward motion.

Driveways to residential properties which have more than three garage units or parking stalls shall be designed such that vehicles can leave and enter the roadway in a forward motion.

#### (b) Location.

Driveways shall be located to provide maximum separation from other drive approaches and roadway intersections to minimize impeding vehicle traffic on the roadway. Minimum and standard dimensions for driveway approach separation, clearances from roadway intersections and offsets at T-intersections or median openings are shown on Figure D-2. The minimum values shown in this figure are the smallest dimensions which shall be permitted. The standard values shall be used to the extent possible within the property frontage.

#### (c) Sight Distance.

Unobstructed sight distances as set forth in Figure SD-3 and SD-4 of APPENDIX A, shall be provided at all driveway approaches for vehicular and pedestrian traffic safety. Fences, walls, signs or other obstructions shall not be placed in the public street and shall not be placed in the sight triangles as set forth in Figures, SD-3, and SD-4. Chain link fences shall not be allowed in front yards.

#### (d) Driveway approach geometry.

Driveway throat widths, return radii or tapers, and angles are shown in Figure D-2 for the various land uses and street classifications. Typical driveway approach details are shown in Figure D-3.

#### (e) Driveway approach grades.

The driveway approach surface shall meet the sidewalk at sidewalk grade. The sidewalk grade shall be established by the Public Works Department or in conformance with approved subdivision layout plan. Normal sidewalk cross-slope through the driveway approach shall be 2%. Where the walk abuts the curb, the cross-slope shall not exceed 2%. No more than 8% slope (ADA) into the street from the outside edge of sidewalk (nearest to street) will be permitted.

### Hastings City Code

The owner or the owner's representative shall notify the Street Superintendent and request grade stakes for the driveway approach and inspection of the work before concrete is poured. The Public Works Department and/or Development Services Department shall be notified at least two working days in advance to give time to schedule any inspections. Work done without prior inspection shall be removed if the completed construction is not in accordance with terms of the permit.

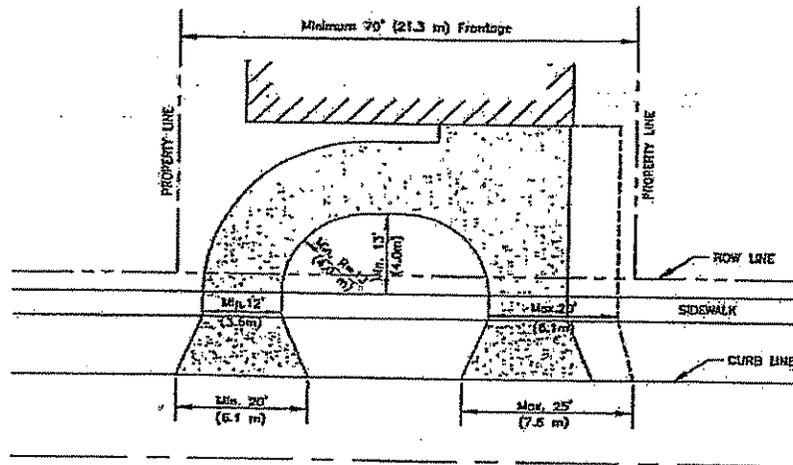
(f) Driveway approach cross-section and construction.

Driveway approaches on paved roadways shall be surfaced with concrete from the roadway edge to the property line. The minimum thickness of driveway approaches for single or two-family dwellings is 6 inches from the roadway edge back to the property line.

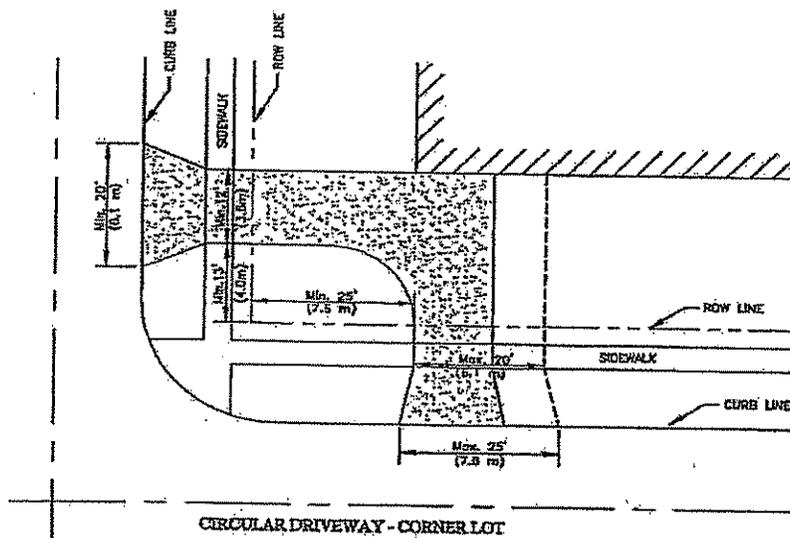
The minimum thickness of commercial driveway approaches is 6 inches from the roadway edge to the property line. The Public Works Department may require a greater thickness for commercial driveways depending on the geometry of the approaches, anticipated traffic volumes and number of trucks using the driveway.

Along roadways improved with a rural-type cross-section and parallel ditch, a drainage culvert shall be installed under the driveway approach. The length, size, grade and location of the culvert shall be determined by the Public Works Department. The culvert is to be purchased by the property owner and installed by the City.

Driveway approaches shall be constructed in conformance with *City of Hastings Standard Specifications for Municipal Construction*.



CIRCULAR DRIVEWAY



CIRCULAR DRIVEWAY - CORNER LOT

NOTE: THE MAXIMUM TOTAL WIDTH OF THE TWO CURB-CUTS FOR A CIRCULAR DRIVEWAY APPROACH SHALL BE 45' (13.7 m)

CIRCULAR DRIVEWAYS  
for One and Two-Family  
Dwelling Units

FIGURE D-1

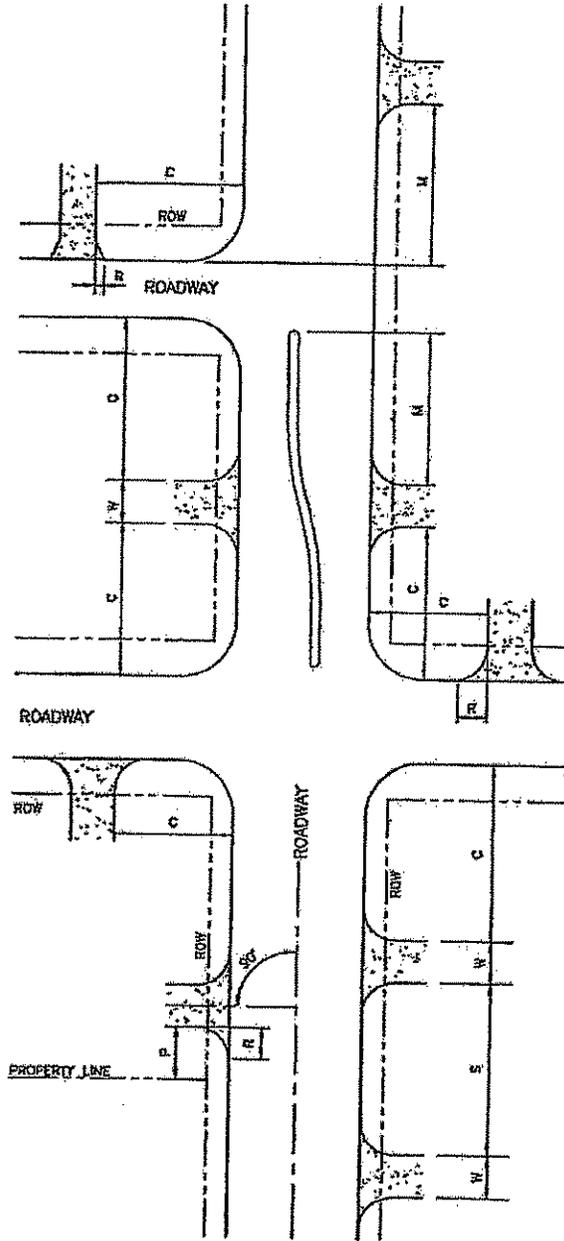


FIGURE D-2

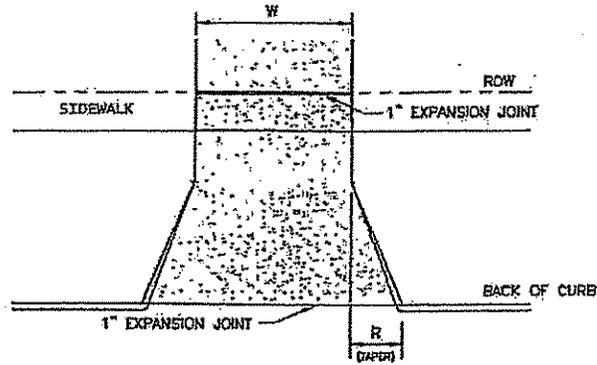
DRIVEWAY DIMENSIONS

	THROAT WIDTH - W		RETURN RADIUS - R	
	TWO-WAY	ONE-WAY	MAJOR ST.	LOCAL/COLL.
Single or Two-Family	10' to 25'	-	2.5' to 5'	2.5' to 5'
Multiple Dwelling	20' to 25'	15' to 20'	15'	15'
Less than 20 Stalls	25'	15' to 20'	15' to 20'	15'
20 or More Stalls				
Commercial	25' to 35'	15' to 20'	15' to 20'	15'
	30' to 40'	20' to 22'	25'	20' to 25'
	30' to 40'	20' to 25'	25'	20' to 25'

DRIVEWAY SPACING & CLEARANCES

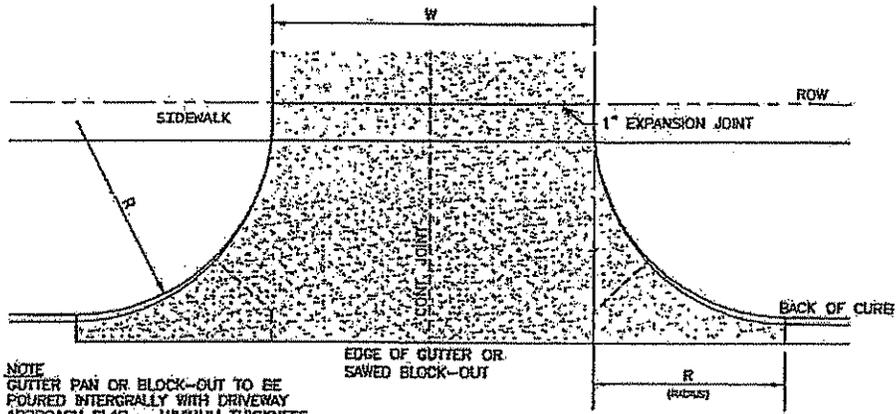
ELEMENT	ROADWAY CLASSIFICATION			
	MINIMUM	STANDARD	MINIMUM	STANDARD
Driveway Separation - S				
Commercial	30'	150' Min.	20'	50'
Single or Two-Family	5'	150' Min.	5'	50'
Intersection Clearances - C				
Major - Major	55'	150' Min.	-	-
Major - Local/Collector	55'	150' Min.	55'	150'
Local/Collector-Local/Coll		150' Min.	42'	55'
T-Intersection or Median				
Opening Offset - M	75'	75'	-	-
Property-Line Offset - P	R	30'	R	10'

# Subdivisions



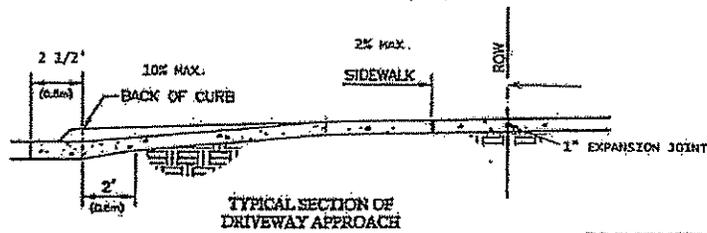
NOTE:  
FOR DIMENSIONS  
"W" AND "R" SEE  
FIGURE D-2

PLAN OF  
RESIDENTIAL DRIVEWAY APPROACH



NOTE  
GUTTER PAN OR BLOCK-OUT TO BE  
FOURED INTEGRALLY WITH DRIVEWAY  
APPROACH SLAB - MINIMUM THICKNESS  
= ROADWAY PAVEMENT THICKNESS TO 2"  
(0.6 m) BACK OF CURB.

PLAN OF  
COMMERCIAL DRIVEWAY APPROACH



TYPICAL SECTION OF  
DRIVEWAY APPROACH

DRIVEWAY DETAILS

FIGURE D-3

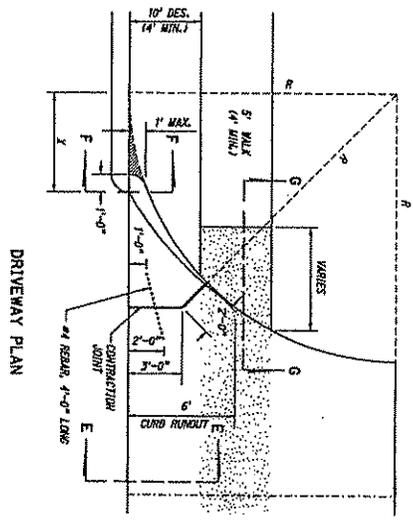
MINIMUM 6" CONCRETE TO PROPERTY LINE

47B OR 5 1/2 SACK AFX (OR AS APPROVED BY CITY ENGINEER)

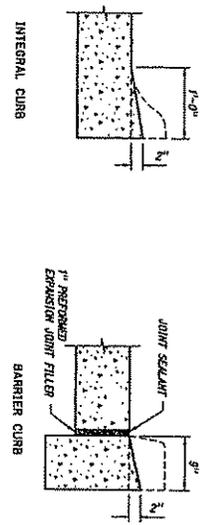
- (1) 10' MIN. IS REQUIRED WHEN THE EXISTING GRADE IS GREATER THAN 4.5%
- (2) 10' MIN. IS REQUIRED WHEN THE EXISTING GRADE IS GREATER THAN 4.5%
- (3) 10' MIN. ROUNDOFF IS REQUIRED WHEN THE EXISTING GRADE IS GREATER THAN 2.2%

10'	1.00'
12'	1.20'
15'	1.50'
20'	2.00'
25'	2.50'
30'	3.00'
35'	3.50'
40'	4.00'

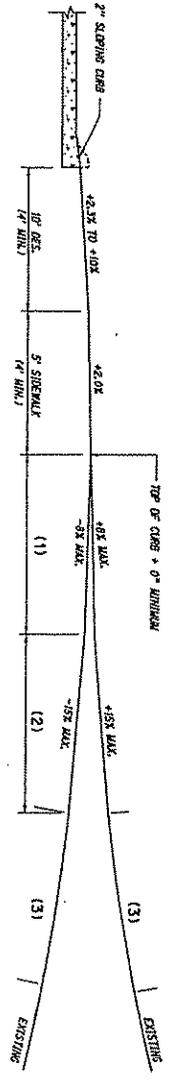
R = RADIUS  
 X =  $(R^2 - T^2)^{1/2}$   
 (X & R IN FEET)



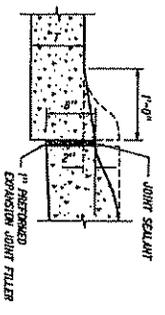
DETAILS OF CURB DROPS



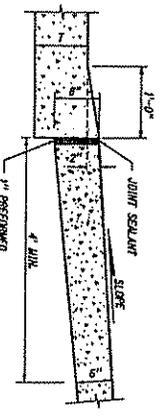
PROFILE URBAN DRIVEWAY WITH SIDEWALK  
 (MAXIMUM PERCENT OF GRADE)



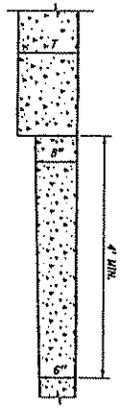
SECTION F--F (URBAN DRIVEWAY)



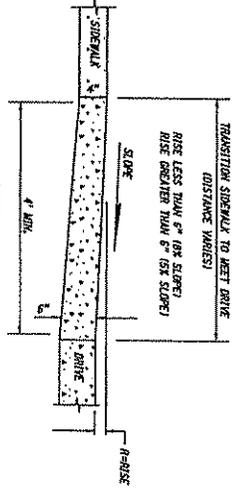
SECTION E--E (URBAN DRIVEWAY)



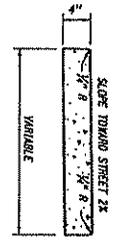
SECTION E--E (RURAL DRIVEWAY)



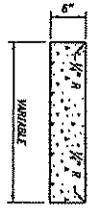
SECTION G--G



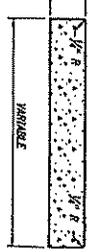
SIDEWALK



GROSSWALK



DRIVEWAY



NOTE:  
 1" PERFORMED EXPANSION JOINT FILLER SHALL BE PLACED IN ALL SIDEWALKS OR GROSSWALKS AT ALL POINTS WHERE SIDEWALKS OR GROSSWALKS ARE ADJACENT TO CURB. IF SIDEWALK OR GROSSWALK TO BE CONSTRUCTED IS LESS THAN 50'-0" IN LENGTH, ONE SUCH EXPANSION JOINT SHALL BE PLACED AS DIRECTED BY THE ENGINEER.

PAVEMENT DETAILS

NEBRASKA DEPARTMENT OF ROADS  
 STANDARD PLAN NO. 301-R10

REVISIONS

NO.	DATE	REVISION

DESIGNED BY: [Signature]  
 DRAWN BY: [Signature]  
 CHECKED BY: [Signature]  
 DATE: [Date]

ORIGINAL  
 JANUARY 31, 1974

(g) Internal circulation.

In order to protect through traffic movement on the roadway, driveways shall project into the property a sufficient distance to provide for the normal turning radius of the types of vehicles to be accommodated. Driveway approach approval will be withheld if parking lots do not conform to the *Parking Lot Design Standards* and do not provide adequate circulation and waiting vehicle storage of drive-in facilities on the property. On-property waiting vehicle storage requirements for the various types of drive-thru facilities are shown in Table VS-1:

Nathan's Curb Grinding  
43370 Rd. 773  
Oconto, NE 68860  
Nathaniel Nixon – 308-330-1921  
Warren Nixon – 308-390-5193  
308-237-5289 – fax