

38-303. Streets, alleys, trails and sidewalks.

(1) Major streets – Compliance with Comprehensive Plan. See Figure 1 Typical Street Sections, Appendix A.

(a) For the purpose of facilitating the movement of traffic, certain streets are designated by the major street plan as major streets. A map showing these major streets will be on file in the office of the Planning Department and reference shall be made to this system of major streets before any preliminary plan is approved.

(b) When a proposed subdivision abuts a major street, or is bounded by a line that will in the future lie in a major street, or is divided by a major street, is shown on the major street plan, then the owner of that subdivision shall dedicate, without charge, any land within such subdivision that is necessary to provide conformity with the major street plan, such dedication to be shown on the preliminary plan and final plat.

(2) Width and gradient.

The minimum width of a street right of way shall be sixty (60) feet and the minimum gradient shall be point five percent (.5%).

(3) Dead-end streets.

Except in unusual cases, no dead-end streets will be approved unless such dead-end streets are provided to connect with future streets in adjacent land, but cul-de-sacs may be permitted where the form or contour of the land makes it difficult to plat with connecting streets. Such cul-de-sacs shall provide proper access to all lots shall not exceed six hundred (600) feet in length, and a turnaround shall be provided at the closed end, with an outside radius of at least fifty-five (55) feet. Except in unusual cases, cul-de-sacs will not be permitted where the turnaround is not clearly visible from the entrance to the street.

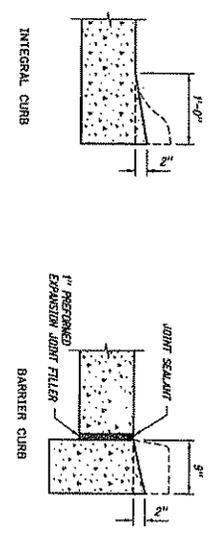
(4) Alleys.

Except in unusual cases, alleys or loading courts shall be provided in business blocks. Alleys are optional in residential districts but should be used for residential formats with narrow lots or building sites, so that vehicle access and parking facilities may be located behind the building lines and streetscapes. Alleys shall have a minimum width of twenty (20) feet of paving for commercial uses, and twenty (20) feet right-of-way with 12 to 16 feet paving for residential uses. In case of intersecting alleys, a cutoff shall be required of at least five (5) feet measured from the point of intersection of the alley lines, the resultant connecting lot line to be at least five (5) feet in length.

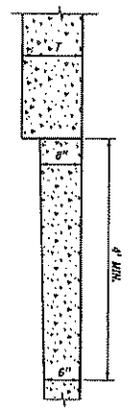
(5) Sidewalks, Trails and Streetscape,

All streets shall have sidewalks in the public right-of-way. Sidewalks shall be a minimum of 4 feet wide on local streets and 5' wide on collector or arterial streets. Trails shall be a minimum of 8 feet to a maximum of 10 feet. Sidewalks shall be on both sides of the street, and located at the right-of-way line. The remaining portion of the right-of-way shall be reserved for a tree terrace separating the sidewalk from moving traffic in the roadway. The tree terrace shall be at least 6 feet wide, except where on-street parking is permitted to support commercial uses, the sidewalk may be expanded to the roadway edge and trees may be planted in a 5 feet by 5 feet tree-well included in the expanded sidewalk area. All sidewalks and trails shall be constructed as part of the roadway paving project.

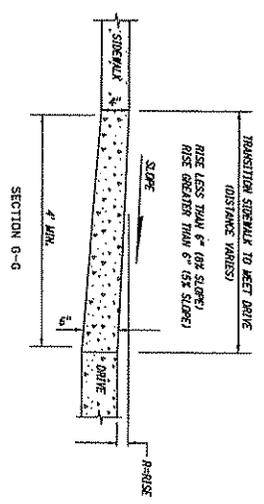
(Ord. No. 4233-11/2009)



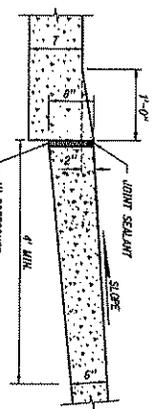
DETAILS OF CURB DROPS



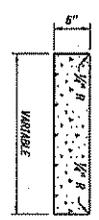
SECTION E-E (RURAL DRIVEWAY)



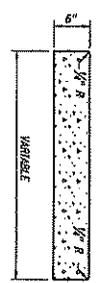
SECTION G-G



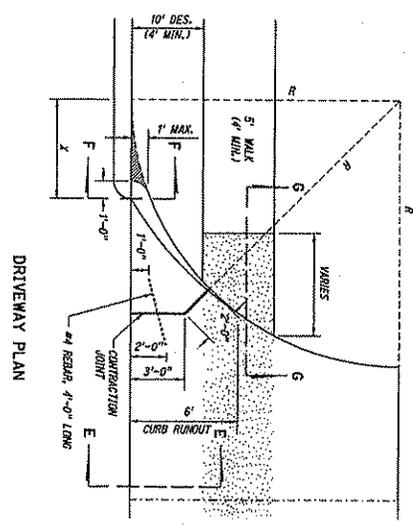
SECTION E-E (URBAN DRIVEWAY)



CROSSWALK



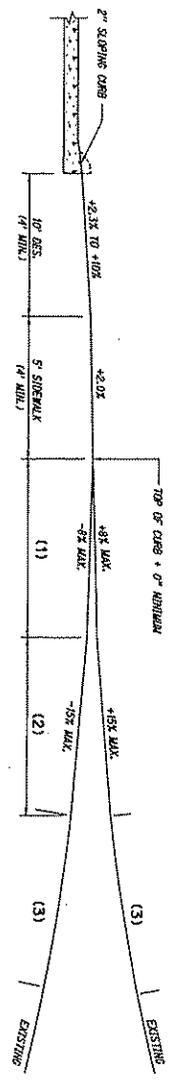
DRIVEWAY



DRIVEWAY PLAN

R	3'-0"	X
5'	4'-0"	X
10'	6'-0"	X
20'	8'-0"	X
30'	10'-0"	X
40'	12'-0"	X

R = RADIUS
X = (R²-1)
(X & R IN FEET)



PROFILE URBAN DRIVEWAY WITH SIDEWALK
(MAXIMUM PERCENT OF GRADE)

NOTE:
1# PREFORMED EXPANSION JOINT FILLER SHALL BE PLACED AT ALL INTERSECTIONS OF DRIVEWAYS AND AT ALL POINTS WHERE SIDEWALKS OR CROSSWALKS ARE ADJACENT TO CURB. IF SIDEWALK OR CROSSWALK TO BE CONSTRUCTED IS LESS THAN 4' WIDE, JOINT FILLER SHALL BE PLACED AS DIRECTED BY THE ENGINEER.

NOTE: Y = PARALLEL THICKNESS

NO.	FILE NO.	ADAPTIVE REVISIONS
01	0100	ADAPTIVE REVISIONS
02	0100	ADAPTIVE REVISIONS
03	0100	ADAPTIVE REVISIONS
04	0100	ADAPTIVE REVISIONS
05	0100	ADAPTIVE REVISIONS
06	0100	ADAPTIVE REVISIONS
07	0100	ADAPTIVE REVISIONS
08	0100	ADAPTIVE REVISIONS
09	0100	ADAPTIVE REVISIONS
10	0100	ADAPTIVE REVISIONS

NEBRASKA DEPARTMENT OF ROADS
STANDARD PLAN NO. 301-R10

PAVEMENT DETAILS

ORIGINAL: JANUARY 21, 1974

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